



## **EXTERNAL MAINTENANCE WORK TO VALLEY TERMINAL BUILDING + GUEST SERVICES BUILDING**

FRIDAY DRIVE, THREDBO VILLAGE  
KOSCIUSZKO NATIONAL PARK NSW 2625  
LOT 862 DP1128686

DATE: SEPTEMBER 2023

PREPARED FOR: KOSCIUSZKO THREDBO C/- CHLOE CHALK

PREPARED BY: ACCENT TOWN PLANNING PTY LTD

REVISION: 01

## EXECUTIVE SUMMARY

The Statement for which this application applies is for external maintenance work to Thredbo Valley Terminal (VT) building and Guest Services building. The purpose of this application is to replace deteriorated external finishes, roof sheets and windows which will improve the longevity of the building, whilst maintaining the original alpine character and design of the building.

This Statement of Environmental Effects (SEE) been amended to capture the inclusion of the Guest Services building in the proposed maintenance works.

The SEE has also been updated to address the additional information request letter by the Department of Planning and Environment dated 26 April 2023. (Their Reference DA23/3863).

The information requested from the Department is as follows:

### Heritage

- a Heritage Impact Statement (HIS) is to be prepared by a suitably qualified and experienced heritage consultant in accordance with section 4.21 of the Precincts – Regional SEPP.

[GBA Heritage Consultants have prepared the following documents for the modified development approval.](#)

- [Concise Statement of Heritage Impact Proposed External Work, Valley Terminal Building, dated 9 June 2023.](#)
- [Addendum to Statement of Heritage Impact Proposed External Work \(Guest Services Building\) dated 10 October 2023.](#)

### Plan Details

- Detailed plans indicating the colour, material and profile of the building elements being removed and also those of the replacement building materials for all external components of the building is to be detailed on a separate plan.
  - [A separated architectural plan has been provided to indicate the colour, material and profile of the building elements removed and replaced.](#)
- A comprehensive Site Environmental Management Plan (including both written and illustrated specifications for SEMP site management).
  - [A comprehensive SEMP is included in Section 7 of this SEE.](#)

## 1.0 INTRODUCTION

The project for which this Statement applies is for a development application for external maintenance work to VT building and Guest Services Building. The building is located in Lot 862 DP1128686, Friday Drive, Thredbo. Thredbo Village is located approximately 35km from Jindabyne, within Kosciuszko National Park, NSW.

Accent Town Planning has been engaged by Kosciuszko Thredbo Pty Ltd (KT), the owners of Thredbo Resort to prepare the Statement of Environmental Effects (SEE) to complement the Development Application (DA) to the NSW Department of Planning and Environment, Alpine Resorts Team, in accordance with the *State Environmental Planning Policy (Precincts – Regional) 2021*, under Part 4 of the *Environmental Planning and Assessment Act 1979* (EP&A Act).

The purpose of this application is to replace deteriorated external finishes, roof sheets and windows which will improve the longevity of the buildings, whilst maintaining the original alpine character and design of the building.

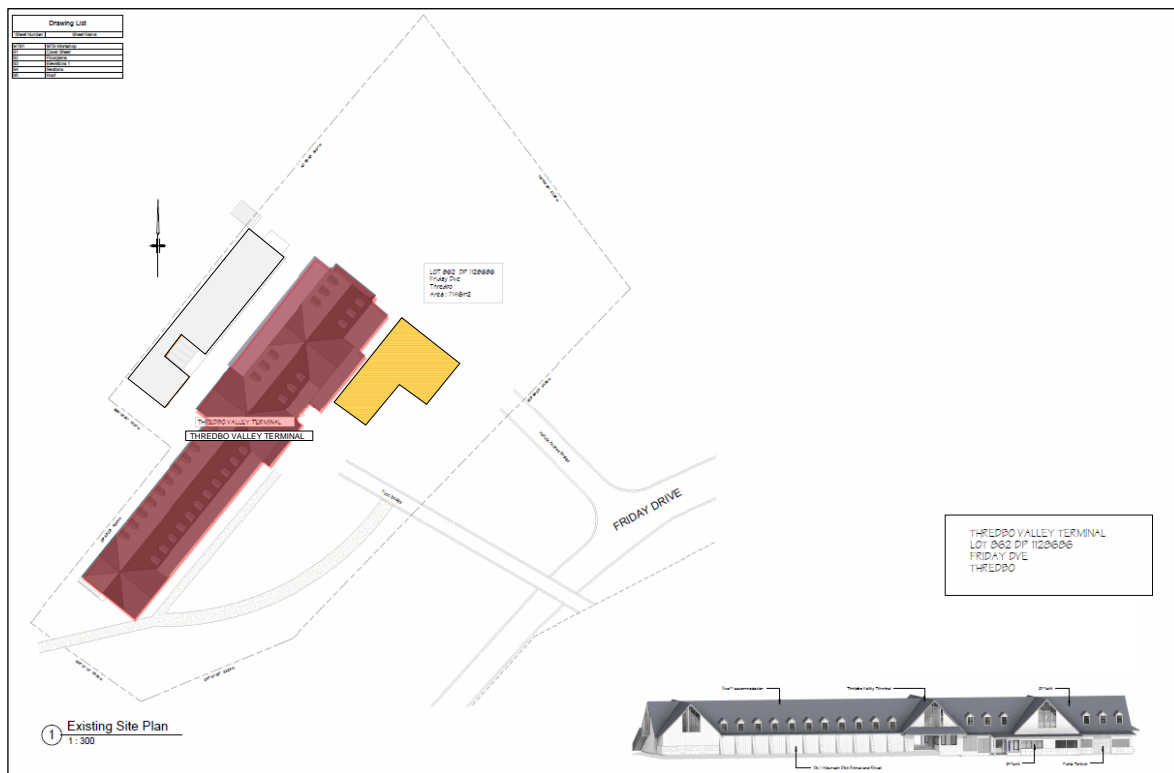
The proposed development is deemed to be of positive influence on the resort through the provision of an enhanced visitor and staff experience. The proposed works will improve the condition of the VT building and ensure its longevity.

There are no proposed adverse effects to the local environment, landscape, streetscape, appearance, or scenic quality of the locality. The existing building footprint will be retained in order to minimise the environmental impact of the proposed improvements. The application of appropriate environmental controls, the potential impacts to the natural environment are negligible.

The VT building was originally constructed in 1960, currently, the ground floor of the building primarily consists of retail/rentals, staff office spaces, a medical facility/practice and public bathrooms. The first floor of the VT building includes a southern wing that consists of staff accommodation including bedrooms, bathrooms, staff kitchen/dining and laundry. The northern wing consists of office spaces varying in size, with a small kitchenette and bathroom.

The Guest services building contains ticket window booths, guest services administration area and staff office space.

**FIGURE 2 VALLEY TERMINAL SITE PLAN**





**FIGURE 1 VALLEY TERMINAL SOUTHERN ENTRY & GUEST SERVICES BUILDING**



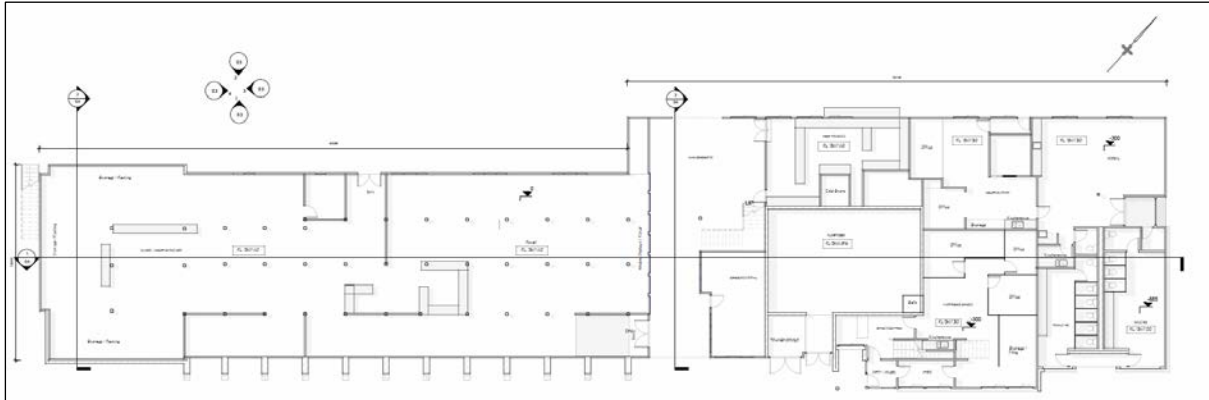
*Photo Guest Service Building*



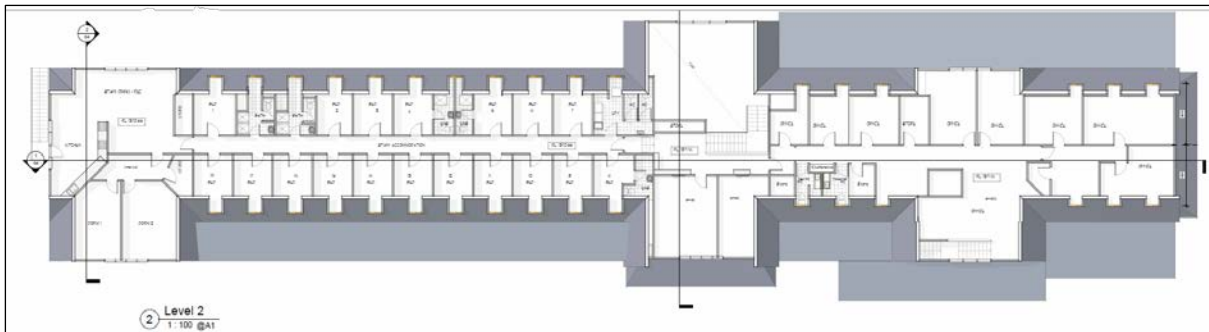


## 2.0 EXISTING BUILDING PLANS

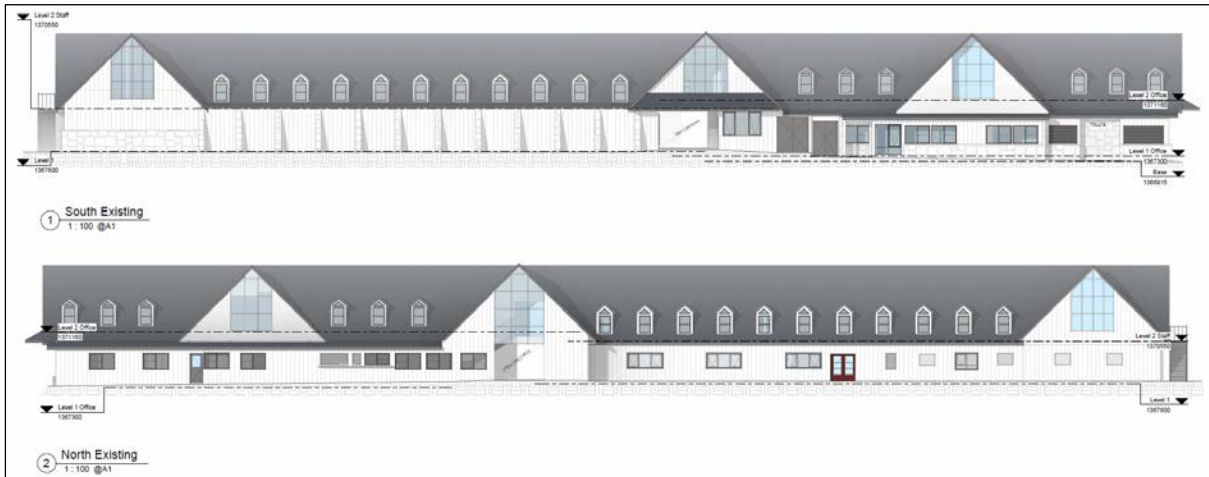
**FIGURE 3 LEVEL 1 FLOOR PLAN**



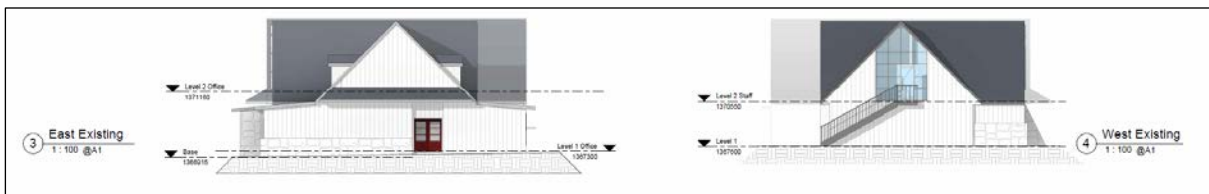
**FIGURE 4 LEVEL 2 FLOOR PLAN**



**FIGURE 5 NORTH AND SOUTH ELEVATION**



**FIGURE 6 EAST AND WEST ELEVATION**



## 4.0 SITE LOCATION & CONTEXT

### 2.1 SITE ANALYSIS

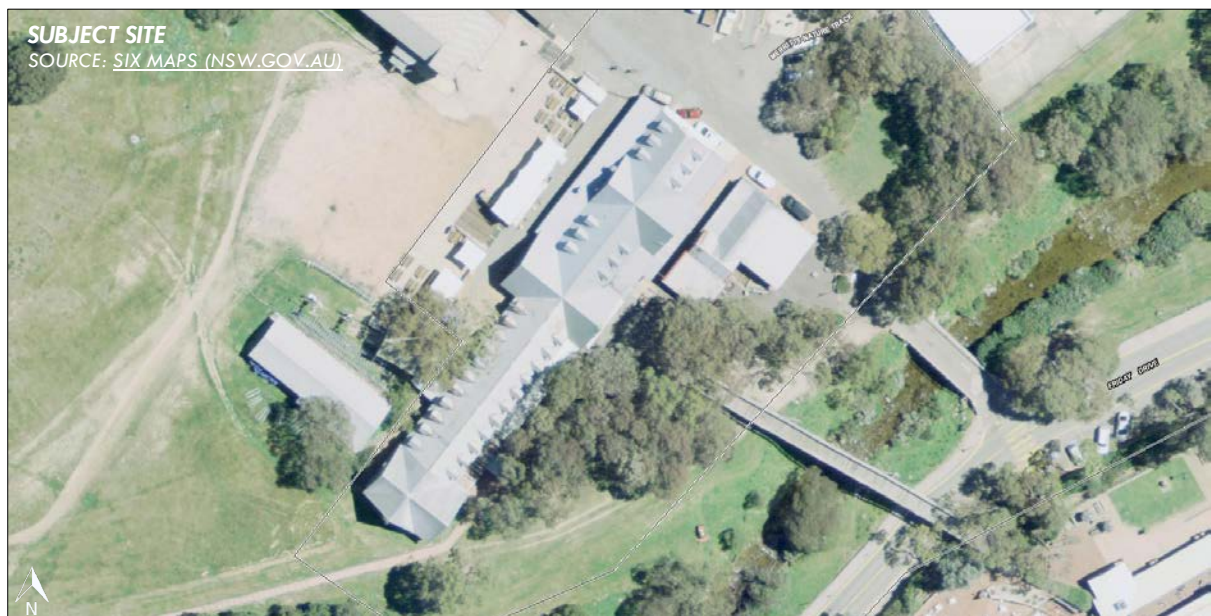
The site is located at Lot 862 DP1128686 Friday Drive, Thredbo within the Kosciuszko National Park. The resort areas, including Thredbo are important to New South Wales due to their economic and social contribution as well as their location within a unique alpine environment. Thredbo is located approximately 35kms from the township of Jindabyne.

The Snowy Mountains is a popular tourist destination for both Australian and international travellers. Kosciuszko National Park offers an array of attractions including, Snow Resorts, world class mountain biking and Australia's highest mountain, Mt Kosciuszko. The local economy is driven by the winter season with skiing and other winter sports being unique to this region of Australia. A growing summer tourism trade has been developing with activities such as hiking, fishing, kayaking, and mountain biking encouraging outdoor enthusiasts to visit the region year-round.

**FIGURE 7 AERIAL VIEW OF SUBJECT SITE**



**FIGURE 8 CLOSE UP VIEW OF SUBJECT SITE**

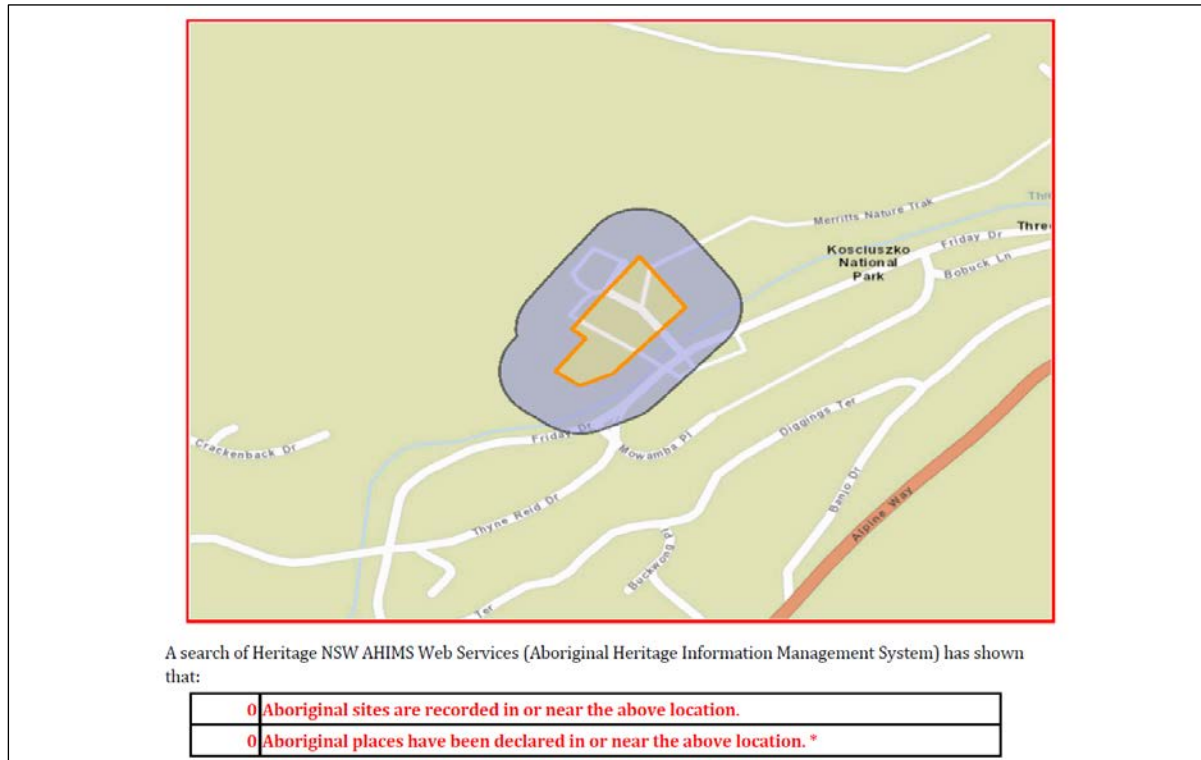




## 2.2 ABORIGINAL HERITAGE

An AHIMS search conducted for Lot 862 DP1128686 on the 03/06/2022 found zero sites or places recorded or declared at the subject allotment. There have been no known Aboriginal artefacts found during previous construction of the existing building and surrounds. External works proposed to Thredbo Valley Terminal will result in no ground surface disturbance and hence the likelihood for Aboriginal artefacts to be found is very unlikely.

**FIGURE 9 AHIMS SEARCH RESULT LOT 862 DP1128686**



Refer to Biodiversity and Aboriginal Heritage Assessment submitted as part of this application.

## 2.3 BIODIVERSITY

Lot 862 DP1128686 does contain Biodiversity Values Map (BVM) areas, as demonstrated in Figure 10 below. The Valley Terminal building is not located within the BVM areas, and the proposed repair works will be occurring to the external building surface only. Hence, the proposed works will not be occurring in BVM areas and will not trigger entry into the Biodiversity Offset Scheme (BOS).

**FIGURE 10 BIODIVERSITY VALUES MAP**



## 2.4 RIPARIAN LANDS AND WATERCOURSES

Lot 862 DP1128686 contains areas identified on the Riparian Lands and Watercourses (RLW) map, as indicated in Figure 11 below.

A small area of the south-western corner of the Valley Terminal Building is located within the RLW map. The RLW map identifies the closest building edge of the Guest Services building is located approximately 25m to the Thredbo River.

The proposed external repair works are minor in nature and will not cause ground disturbance, nor will they negatively impact upon the Thredbo River based upon the minor scale of works.

The DPE Water Waterfront Land e-tool was also used to determine whether the proposed works will require controlled activity approval licence. The Waterfront Land e-tool determined that the proposed works do not require a controlled activity approval licence. See Appendix I for the Waterfront Land e-tool results.

**FIGURE 11 RIPARIAN LANDS AND WATERCOURSES MAP**



## 2.5 SITE CONTOURS

Landfall is generally consistent across the site, there is a significant upslope to the North-west, and a consistent downslope towards Thredbo River. The slope continues upward to the South-east from Friday Drive through to the Alpine Way.

**FIGURE 12 CONTOUR PLAN**





No excavation works or ground disturbance is proposed as part of this application and therefore there will be no geotechnical issues or impacts associated with the project, and hence, a geotechnical report will not be required to be submitted with the application.

## 2.6 COLOUR SCHEDULE

No changes are proposed to the existing colours of the Valley Terminal & Guest Service Buildings. Any roof sheeting, cladding and windows that requires repair/replacement will be like-for-like to match the existing building.

**FIGURE 13 PROPOSED COLOURS**



### COLOURS:

WALL CLADDING TO MATCH  
COLORBOND "EVENING HAZE"  
WHICH IS INTENDED TO MATCH  
EXISTING



FASCIA AND WINDOW FRAMES TO  
MATCH COLORBOND "MANOR  
RED"



ROOFING TO BE GALVANISED  
STEEL

## 2.7 BUILT FORM AND CHARACTER OF ADJOINING DEVELOPMENT

The Valley Terminal building is situated in the heart of Thredbo Village and has played a key role in establishing Thredbo as a popular ski tourist destination.

Surrounding developments are similar in scale and design, offering architectural features including natural stone, loft gable rooflines, feature articulation in structural elements and painted timber cladding. The surrounding buildings are of various architectural vernacular styles - typically built 40 years or more ago with a growing number of new modern style buildings.

There will be no change to the current view sharing as there are no major changes to the exterior of the building or windows with all alterations remaining within the existing building footprint.

### 3.0 PROPOSED MAINTENANCE WORK

**FIGURE 13 PROPOSED WORK AREAS**



The proposed maintenance work on the main Valley Terminal will include:

- The removal of existing deteriorated native hardwood cladding and replacement with like-for-like product.
- The removal of existing deteriorated corrugated metal roof sheeting and replacement with like-for-like product.
- The removal of windows and replacement with like-for-like product.

The proposed maintenance works on the Guest Services building will include:

- Replacement of the existing fixed glass windows within the existing timber frame with new double-hung windows for five ticket offices within the Guest Service building.
- Removal and replacement of the timber ceiling with a like-for-like lining product to the external awning of the Guest Service building.
- Removal of existing My Thredbo Kiosk signage above the existing ticket windows and installation of 5 screens above the windows.

It is important to note, that the proposed building works for VT & Guest Service buildings will be within the existing footprint.

The proposed development is deemed to be of positive influence on the resort through the provision of an enhanced staff and visitor experience through the proposed repair and upgrade work to the VT building.

The external repair works will improve the condition of the VT building and will therefore improve its longevity. The proposed works are consistent with the character and built environment of Thredbo Village, as they aim to retain the appearance and condition of the building.



### 3.1 EXTERNAL CLADDING

The external cladding of the Valley Terminal building is currently in a state of disrepair resulting from the harsh alpine conditions, and prolonged exposure to moisture along the base of the building (as demonstrated in section 2.8 of this report). Repairs are urgently needed to ensure the condition of the building is improved to ensure the longevity of the structure. This application is seeking approval for the entirety of the building, to allow Kosciuszko Thredbo to replace deteriorated cladding for the entirety of the building as needed.

The proposed like-for-like cladding will match the existing cladding so as to not impact upon the heritage significance of the building and ensure the fabric and design of the building is retained.

**FIGURE 14 PROFILE OF EXISTING CLADDING TO VALLEY TERMINAL BUILDING**



**FIGURE 15 CEILING LINING TO GUEST SERVICES BUILDING AWNING**



### 3.2 ROOF SHEETING

The existing roof sheeting of the Valley Terminal building has also deteriorated in some areas due to its age, and the harsh alpine weather conditions. For this reason, areas of the roof require replacement to prevent damage to the internal roof cavities (including structural beams, insulation gyprock ceiling) and to ensure the longevity of the building. This application is seeking approval for the replacement (like-for-like) of roof sheeting for the entire building, to be replaced as required.

The proposed like-for-like roof sheeting will match the existing roofing so as to not impact upon the heritage significance of the building and ensure the fabric and design of the building is retained.

**FIGURE 16 EXISTING ROOF CLADDING TO VALLEY TERMINAL BUILDING**



### 3.3 WINDOWS

As identified within section 3.4 of this report, many of the existing windows in the Valley Terminal building have deteriorated significantly due to age and weathering. All windows in the Staff accommodation wing of the VT building require urgent replacement, and windows in other areas of the building are of a similar age and are anticipated to require replacement soon. For this reason, this application of seeking to replace (with like-for-like) all windows within the Valley Terminal building (excluding windows that have been recently replaced). It is proposed that the windows that need replacing will be done as required.

The proposed like-for-like windows will reflect a similar design to what is existing, in order to retain the fabric and design of the building. An example of the replacement windows is demonstrated in the figure below, where older windows have been replaced above the Avalanche seating area.

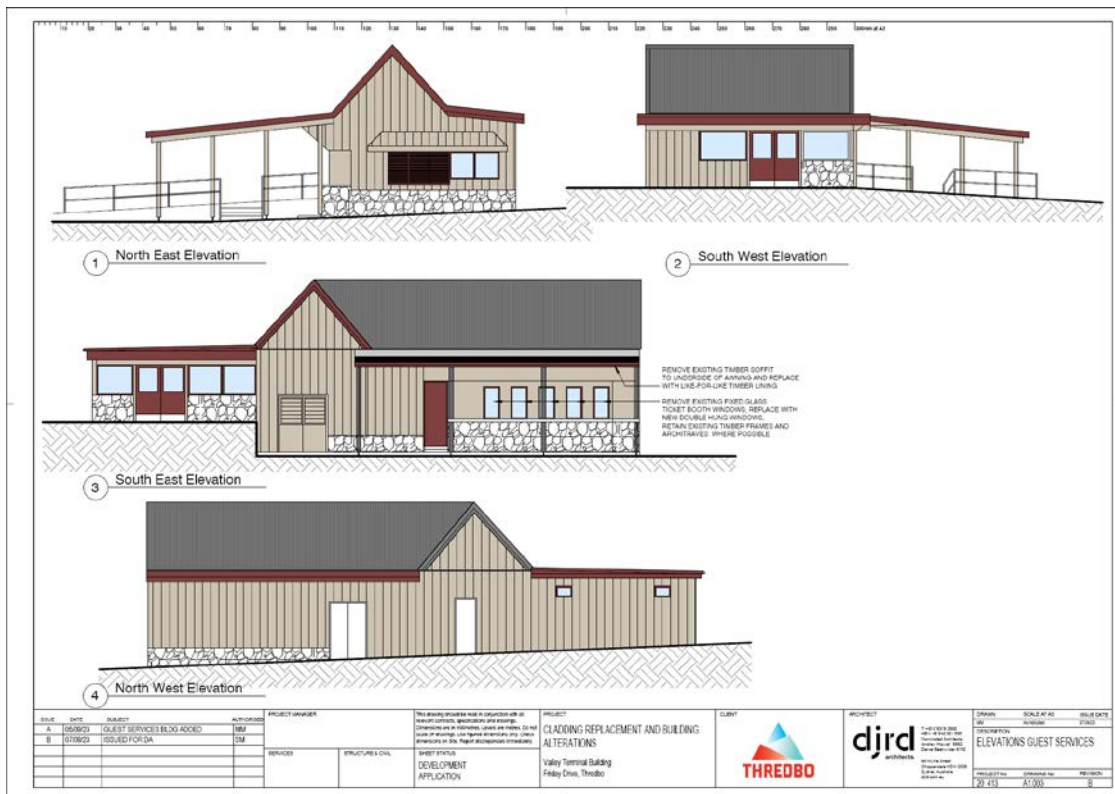
**FIGURE 17 NEW WINDOW ABOVE AVALANCHE SEATING AREA**





**FIGURE 18 VALLEY TERMINAL BUILDING: PROPOSED REPAIRS & MAINTENANCE: ELEVATION PLANS**





### 3.4 SITE PHOTOS – VALLEY TERMINAL BUILDING: PROPOSED REPAIRS & MAINTENANCE





CLADDING OUTSIDE OF RENTALS



FACING AVALANCHE CAFE



EXTERNAL WALL OF RETAIL STORE





**SOUTH END OF STAFF ACCOMMODATION - ENTRY**



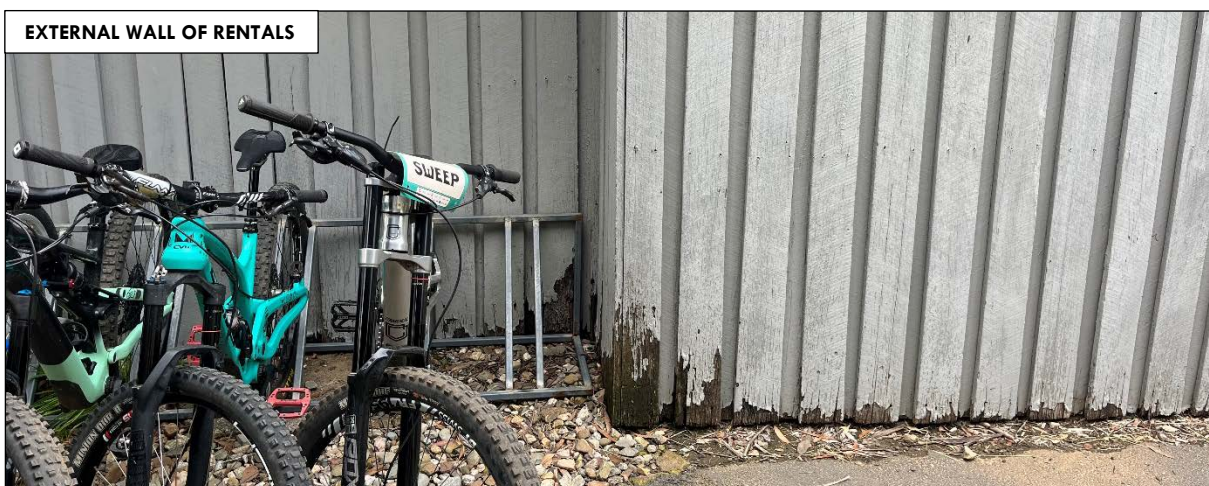
**WALL BELOW STAFF ACCOMMODATION ENTRY**



**EXTERNAL WALL OF RENTALS**









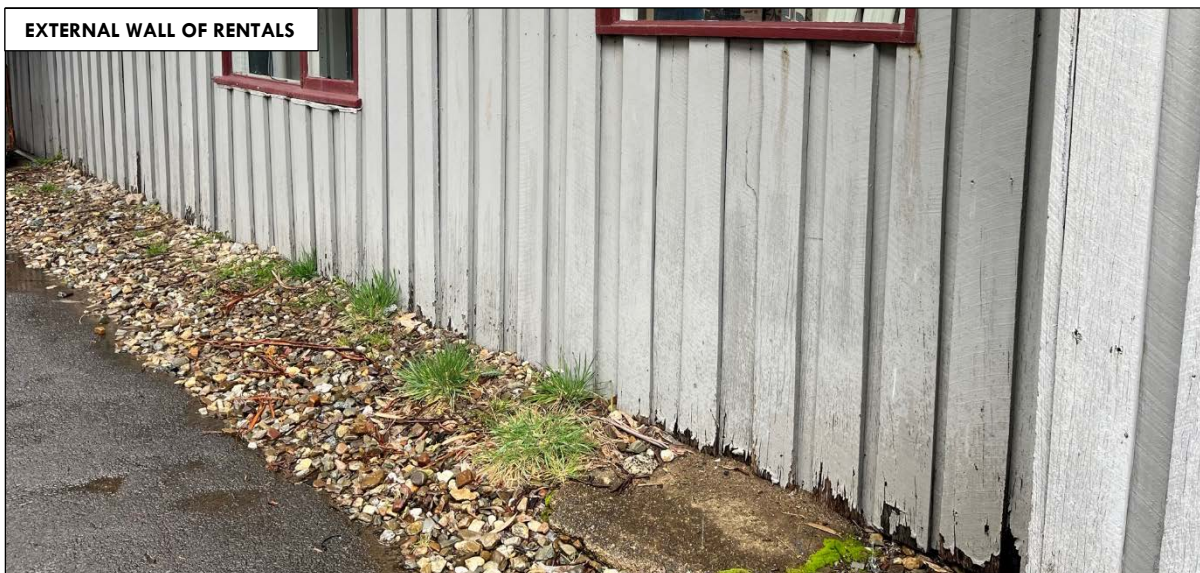
EXTERNAL WALL OF SKI PATROL & ENVIRO SERVICES OFFICE



NORTH-WEST CORNER NEAR RACE CLUB FACING TOWARD RENTALS



EXTERNAL WALL OF RENTALS





**WALL SOUTH OF AVALANCHE SEATING AREA**



**NORTHERN WINDOWS**



**SOUTH-EAST CORNER (RIVER SIDE) STAFF ACCOMMODATION WINDOWS**

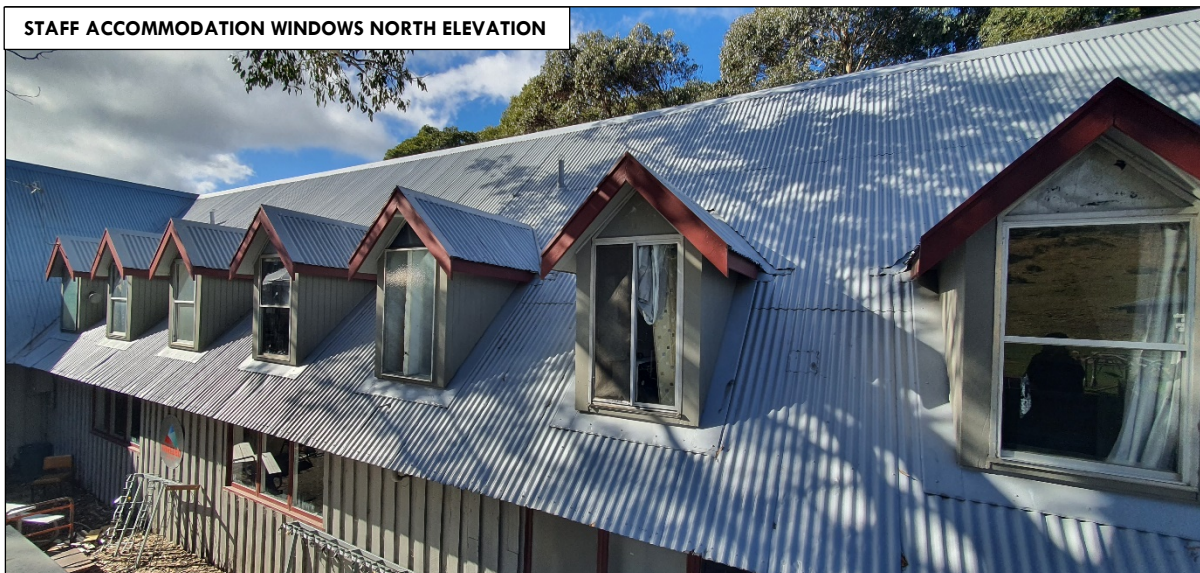




WINDOWS ABOVE SOUTHERN ENTRANCE



STAFF ACCOMMODATION WINDOWS NORTH ELEVATION





**SOUTH-WESTERN CORNER OF VALLEY TERMINAL**



### 3.5 SITE PHOTOS – GUEST SERVICES BUILDING: PROPOSED REPAIRS & MAINTENANCE



*Guest Services Building*





*Guest Services Building*



*Ticket windows to be replaced within the existing timber frames.*





*Ceiling Lining to be Replaced with Like-for-Like Material to Guest Services Building Awning.*

## 4.0 GENERAL INFORMATION

### PROJECT DESCRIPTION

The project for which this Statement applies is for external maintenance works to the Valley Terminal Building, replacement of ticket office windows and replacement of the ceiling lining to the guest service building.

### SITE SUITABILITY

The site is suitable for the proposed development.

<ul style="list-style-type: none"> <li>site constraints such as flooding, slope, geotechnical hazards, bushfire, and any other risks.</li> </ul>	<p>The subject site is identified as bushfire prone land, and hence a Bushfire Hazard Assessment Report has been prepared and will be submitted as part of this application.</p> <p>The subject site is not identified as flood prone land.</p> <p>The proposed works are for repair and replacement works only and will not result in ground disturbance and will only be occurring to existing parts of the VT&amp; Guest Service Building. Hence, a geotechnical report will not be required as part of this application.</p>
<ul style="list-style-type: none"> <li>effects on the local environment, landscape, streetscape, appearance, or scenic quality of the locality.</li> </ul>	<p>There are no proposed adverse effects of the local environment, landscape, streetscape, appearance, or scenic quality of the locality as the proposed development is consistent with surrounding development and existing buildings in Thredbo. The proposed works are for repair and replacement only.</p>

<ul style="list-style-type: none"> <li>biological and ecological impacts including the impacts on fauna and flora.</li> </ul>	<p>The subject lot does contain areas of High Biodiversity Values. However, the proposed works will not be occurring in areas identified on the Biodiversity Values Map (BVM). The repair and replacement works will occur to the existing buildings only and will not negatively impact upon the BVM areas. No ground disturbance will be occurring as part of this proposal. Caution will be taken in handling and disposing of any waste materials associated with the proposed works, to limit any potential damage or contamination of these areas.</p>
<ul style="list-style-type: none"> <li>impacts on existing and future amenity of the locality.</li> </ul>	<p>The proposed works are deemed to be of positive influence on both the existing building as well as Thredbo Village, by offering a higher level of staff accommodation which will enhance the tourism service industry in the area.</p>
<ul style="list-style-type: none"> <li>the age and condition of any structures or buildings.</li> </ul>	<p>Thredbo Valley Terminal was constructed in the 1960's. Since construction, the building has deteriorated in the highlighted areas, hence the need for repairs and upgrades.</p>
<b>PRESENT AND PREVIOUS USES</b>	
<p><i>"The Valley Terminal building was a central development for the early head lessees, being the public area where all skiers passed through to gain access to the ski slopes"</i> (OzArk, Historic Heritage Assessment, 2021). Today, the VT building offers retail spaces for the public, and a range of office spaces for Thredbo staff, alongside staff accommodation.</p>	
<b>OPERATIONAL DETAILS</b>	
<p>The building includes retail space, a café, a medical practice, office spaces as well as staff accommodation. There are no proposed changes to the current operational details for the VT or guest service buildings.</p>	
<b>CHANGE OF USE OF A BUILDING (WHERE THERE IS NO BUILDING WORK)</b>	
Not applicable.	
<b>BUILDING CLASSIFICATION AND BUILDING CODE OF AUSTRALIA (BCA)</b>	
<p>The VT &amp; Guest Service buildings includes class 3, 5 and 6 building classes.</p>	
<b>SNOW DEPOSITION</b>	
<p>The roof design of the VT building is appropriately sloped, and directs accumulated snow to fall away from entrances, establishing safe entry and exit into and out of the VT building.</p>	
<b>GEOTECHNICAL ENGINEERING SUMMARY</b>	
Not applicable, no ground disturbance will occur as part of this application.	
<b>STRUCTURAL ENGINEERING DETAILS</b>	
No structural work is proposed as part of this application.	
<b>SOCIAL AND ECONOMIC IMPACT</b>	
<p>The proposed maintenance works are expected to have positive social and economic impacts. The VT is recognised as one of the original buildings within Thredbo Village that helped pave the way for Thredbo to become what it is today. The maintenance of the building will improve the longevity of the building and ensure the continuation and operation of businesses; hence the works will contribute to the continued economic function of Thredbo Village. The significance of the building will also be maintained; hence it is crucial that these works are undertaken urgently to prevent further deterioration and damage to the building.</p> <p>Additionally, the maintenance of the VT building will secure the long-term resilience of the resort and will protect the seasonal jobs offered annually each ski season.</p>	



**ACCESS AND TRAFFIC**

Thredbo Village is accessible by car on a year-round basis. The proposed development does not include the addition of any bed licences or vehicle parking areas and is not expected to generate additional traffic to the resort. For this reason, the proposed development will have no impact on the existing access and traffic to Thredbo Village.

**PRIVACY, VIEWS AND OVERSHADOWING**

There are no proposed changes to the size, layout and/or footprint of the VT building, and the maintenance work is for direct like-for-like replacement. Hence, the proposed works are not anticipated to have any negative impacts to privacy, views and overshadowing.

**AIR AND NOISE****AIR**

Works involving dust dispersion will use water spray to help keep material damp and dust down. Covers will be placed over waste storage areas and piles of excavated materials to prevent dust dispersion. When transporting materials that cause dust they will be dampened and covered before moving.

**NOISE**

Noise on a construction site can become a form of pollution to the local environment through the use of plant, machinery and tools. For protection of employees and visitors to the site they are issued with PPE including ear protection.

To reduce noise pollution from site the following procedures will be followed:

- All plant, machinery and tools will be maintained in good working order at all times.
- Work involving noisy tools or machinery to be used inside the building structure when possible.
- Strict hours of operation for each site will be implemented to reduce noise pollution to the surrounding areas.
- In the instance of receiving a complaint in regard to noise levels immediate rectification will occur as far as practical.

**SOIL, WATER AND WASTEWATER MANAGEMENT**

The proposed works do not involve any ground disturbance, and there are no proposed changes to existing water and wastewater management.

**HERITAGE**

The Valley Terminal Building is identified as a Heritage Item under *Schedule 4 Heritage Items – Chapter 4 of the Precincts Regional SEPP 2021*.

The proposed works will have an acceptable heritage impact, given that the significance of the VT building and its ability to contribute to the Thredbo Village will be retained. The proposed maintenance works will have no adverse impact on the heritage significance of the building, in fact the works will improve the condition, and therefore longevity of the building to ensure the continuation of its operation and significance.

The removal of some fabric is deemed as acceptable, given that the proposed maintenance work will sympathetically replace existing deteriorated fabric, with a new like-for-like product to match the existing in terms of profile, material, colour and finishes.

Please refer to the following documents prepared by GBA Heritage Consultants for the modified development approval.

- Concise Statement of Heritage Impact Proposed External Work, Valley Terminal Building, dated 9 June 2023.
- Addendum to Statement of Heritage Impact Proposed External Work (Guest Services Building) dated 10 October 2023.

Please refer to the Biodiversity and Aboriginal Heritage Assessment submitted as part of this application.

<b>ABORIGINAL CULTURAL HERITAGE</b>
An AHIMS search for Lot 862 DP1128686 found zero Aboriginal sites and places recorded in the lot. There have been no known Aboriginal artefacts found during previous construction of the VT building and surrounding areas. The proposed works do not include any ground disturbance, hence there is no potential for Aboriginal artefacts to be found as part of the proposed works.
<b>VEGETATION REMOVAL</b>
Not applicable, the proposed works are maintenance only, and do not require the removal of any vegetation.
<b>ENERGY</b>
The maintenance works will include the removal of deteriorated external materials to be replaced with like-for-like materials. Hence, the energy efficiency/rating of the building is not anticipated to change. If required, a Section J assessment can be undertaken during construction certificate.
<b>WASTE</b>
<p>The proposed development is not expected to increase visitation numbers to the resort. As a result, there will be no need to increase the capacity of existing waste disposal facilities due to the proposed development.</p> <p>Waste generated from the removal of the deteriorated materials and the general construction waste and other waste associated with the maintenance work to the VT building will be disposed of in the skip bin and deposited in the nearest council waste facility.</p>
<b>DEMOLITION</b>
Waste generated from the proposed maintenance works will be disposed of in the skip bin and deposited in the nearest council waste facility.
<b>TERMITE PROTECTION</b>
To be conditioned as part of the Development Application consent.
<b>SCHEDULE OF WORKS</b>
Construction will be during the summer season from October to May with no works proposed to be conducted during the winter season June to September. The start date for construction is not yet set and the year will be determined after development consent has been issued.

## 5.0 PERMISSIBILITY, LEGISLATION AND REGIONAL PLAN

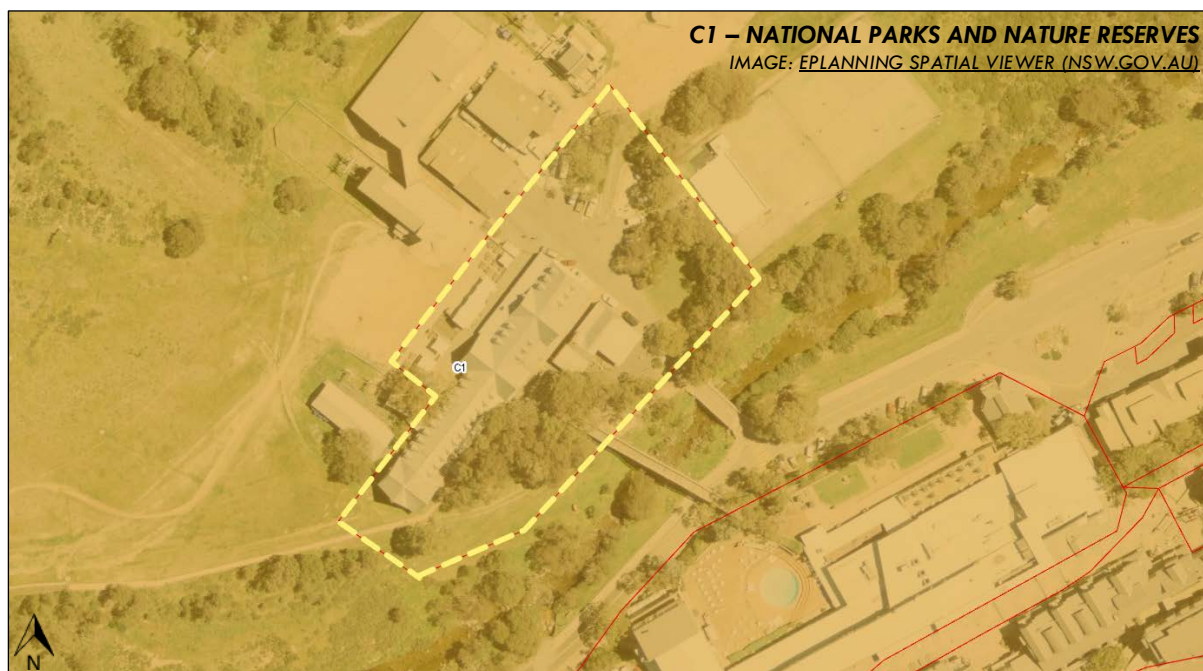
### 5.1 SOUTH EAST AND TABLELAND REGIONAL PLAN 2036

The proposed upgrades to Thredbo Village will improve the longevity of the VT building by maintaining and replacing external materials that are deteriorated. This is consistent with the purpose of the Regional Plan, which offers a framework to provide infrastructure and development to help strengthen the long-term resilience of the South-East and Tableland region. The regional Plan seeks to acknowledge the unique environmental and cultural significance of Kosciuszko National Park, whilst also enhancing visitor experience, which this application has taken into consideration.

### 5.2 STATE ENVIRONMENTAL PLANNING POLICY (PRECINCTS – REGIONAL) 2021

The proposal is for external maintenance works to the existing Valley Terminal Building. The proposed maintenance works are permissible under Chapter 4 of the State Environmental Planning Policy (Precincts – Regional) 2021, as the works help to maintain the condition of the existing Valley Terminal Building, and do not propose any changes to the existing permissible uses of the building.



**FIGURE 14 LAND ZONING MAP****THREDBO ALPINE RESORT****1 Permitted without consent**

Nil

**2 Permitted with consent**

Advertising structures; Building identification signs; Business identification signs; Car parks; Commercial premises; Community facilities; Depots; Eco-tourist facilities; Emergency services facilities; Entertainment facilities; Environmental facilities; Environmental protection works; Fences; Function centres; Helipads; Information and education facilities; Infrastructure facilities; Lifting facilities; Management trails; Medical centres; Monitoring stations; Places of public worship; Public utility undertakings; Recreation facilities (indoor); Recreation facilities (outdoor); Recreation infrastructure; Ski slope huts; Ski slopes; Snow-making infrastructure; Staff accommodation; Telecommunication facilities; Tourist and visitor accommodation; Transport depots; Vehicle repair stations

**3 Prohibited**

Bed and breakfast accommodation; Farm stay accommodation; Any other development not specified in item 1 or 2

**5.2.1 AIMS & OBJECTIVES OF CHAPTER (PART 4.1 PRECINCTS – REGIONAL SEPP)**

(1) The aim of this Chapter is to protect and enhance the Alpine Region by ensuring development is managed with regard to the principles of ecologically sustainable development, including the conservation and restoration of ecological processes, natural systems and biodiversity.	Noted. The proposed works are consistent with the aims and objectives of the policy as the development will improve the condition and therefore longevity of the building.  The proposal will create positive social and economic impacts and works towards securing the long-term resilience of the alpine resort.
(2) The objectives of this Chapter are as follows— (a) to encourage the carrying out of a range of development to support sustainable tourism in the Alpine Region all year round, if the development does not result in adverse environmental, social or economic impacts on the natural or cultural environment of the Alpine Region, including cumulative impacts	Complies, not adverse impacts will result from this application. The proposed maintenance works to the VT & Guest Services buildings will be of positive influence to Thredbo Village and will have no negative impact on existing summer and winter transport, reticulated effluent management, waste disposal or water supply. The proposed development will not generate additional visitation to the resort and hence will



<p>on the environment from development and resource use,</p> <p>(b) to establish planning controls that—</p> <p>(i) contribute to and facilitate the carrying out of ecologically sustainable development in the Alpine Region, and</p> <p>(ii) recognise the Alpine Region's significant contribution to recreation and the tourism economy in the State,</p> <p>(c) to minimise the risk to the community of exposure to environmental hazards, particularly geotechnical hazards, bush fires and flooding, by—</p> <p>(i) generally requiring development consent on land in the Alpine Region, and</p> <p>(ii) establishing planning controls for buildings to ensure the safety of persons using the buildings if there is a fire.</p>	<p>have no impact on the existing capacity of the reticulated effluent management system.</p> <p>Likewise, with no increase in visitation there will be no need to increase the capacity of existing waste disposal facilities or water supply. The proposed works are expected to have minimal environmental impact, based upon the scale of the works, and the maintenance works will be occurring to an existing building. With the implementation of appropriate environmental controls during construction, the potential impacts from the works on the natural environment are negligible.</p> <p>The proposed development has been designed so as to not disturb or detriment the surrounding natural environment. This includes the consideration of bushfire threat and geotechnical considerations. See Bushfire Hazard Assessment Report (BHAR) submitted as part of this application.</p>
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## 5.2.2 OTHER DEVELOPMENT CONTROLS (PART 4.4 PRECINCTS – REGIONAL SEPP)

4.21 Heritage Conservation	
<p>(1) The objective of this section is to conserve—</p> <p>(a) the environmental heritage of the Alpine Region, and</p> <p>(b) the heritage significance of heritage items, including associated fabric, settings and views, and</p> <p>(c) Aboriginal heritage items and Aboriginal places.</p>	<p>The Valley Terminal Building is identified as a Heritage Item under <i>Schedule 4 Heritage Items – Chapter 4 of the Precincts Regional SEPP 2021</i>.</p> <p>The proposed works will have an acceptable heritage impact, given that the significance of the VT building and its ability to contribute to the Thredbo Village will be retained. The proposed maintenance works will have no adverse impact on the heritage significance of the building, in fact the works will improve the condition, and therefore longevity of the building to ensure the continuation of its operation and significance.</p> <p>Please refer to the following documents prepared by GBA Heritage Consultants for the modified development approval.</p>
<p>(2) Development consent is required for the following in the Alpine Region—</p> <p>(a) demolishing or moving a heritage item,</p> <p>(b) altering a heritage item, including by doing the following to a heritage item that is a building—</p> <p>(i) making changes to the detail, fabric, finish or appearance of the building's exterior,</p> <p>(ii) making structural changes to the building's interior,</p> <p>(c) disturbing or excavating land that is, or contains, an Aboriginal heritage item,</p> <p>(d) erecting a building on land that is, or contains, a heritage item,</p> <p>(e) subdividing land that is, or contains, a heritage item.</p>	<p>This DA seeks approval for the external maintenance work to the Valley Terminal.</p>

(3) Development consent is not required under this section for the following development— (a) development that involves only the removal of a tree or other vegetation that the consent authority is satisfied is a risk to human life or property, (b) exempt development, (c) development that does not require development consent under section 4.14.	N/A
(4) The consent authority may, before work is carried out, give written notice to an applicant for development consent that development consent is not required under this section if the consent authority is satisfied the development— (a) is of a minor nature or is for the maintenance of the heritage item, and (b) the development will not adversely impact the heritage significance of the heritage item.	noted
(5) In deciding whether to grant development consent for development under this section, the consent authority— (a) must consider the effect of the development on— (i) the heritage significance of the heritage item, and (ii) Aboriginal objects known or reasonably likely to be located on the land, (b) may require the submission of a heritage conservation management plan, and (c) for development on land that is, contains or is near a heritage item— may require the preparation of a heritage impact statement.	<p>The proposed maintenance works will have no adverse impact on the heritage significance of the building.</p> <p>Please refer to the following documents prepared by GBA Heritage Consultants for the modified development approval.</p> <p>The works will not require ground surface disturbance and hence the likelihood for Aboriginal artefacts to be found is very unlikely</p> <p>See Biodiversity, Aboriginal &amp; Cultural Assessment.</p>
(6) Development consent must not be granted to development on land that is, or contains, an Aboriginal heritage item, and that requires development consent under this section, unless the consent authority has— (a) given written or other appropriate notice of the development to the local Aboriginal communities, and (b) considered responses received from the communities within 28 days after the notice is given.	See Biodiversity, Aboriginal & Cultural Assessment.
4.22 Conservation incentives	
(1) This section applies to development in the Alpine Region that— (a) involves a building that is a heritage item, or (b) is on land on which a building that is a heritage item is located.	<p>VT is identified as a Heritage Item Schedule 4 Heritage Items of the SEPP.</p> <p>Please refer to the following documents prepared by GBA Heritage Consultants for the modified development approval.</p>
(2) Development consent may be granted to development to which this section applies, even if the development would otherwise be prohibited under this Chapter, if the consent authority is satisfied of the following—	The removal and replacement of the existing deteriorated native hardwood cladding, metal roof sheeting and windows and replacement with like-for-like products will not impact the heritage value of the building but facilitate the



<p>(a) granting the development consent will facilitate the conservation of the heritage item,</p> <p>(b) the development will be in accordance with a heritage conservation management plan that has been approved by the consent authority,</p> <p>(c) the development consent will require the carrying out of all necessary conservation work identified in the heritage conservation management plan,</p> <p>(d) the development will not adversely affect the heritage significance of the heritage item, including its setting,</p> <p>(e) the development will not have a significant adverse impact on the amenity of the surrounding area.</p>	<p>conservation of the Valley Terminal building as a heritage item.</p> <p>Please refer to the following documents prepared by GBA Heritage Consultants for the modified development approval.</p> <p>See Biodiversity, Aboriginal &amp; Cultural Assessment.</p>
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### 5.2.3 DEVELOPMENT ASSESSMENT & CONSENT (PART 4.5 PRECINCTS – REGIONAL SEPP)

4.26 Master plans	
(1) The Minister must prepare and approve a master plan that applies to the Alpine Region.	noted
<p>(2) The master plan must contain the following information—</p> <p>(a) the strategic vision and general objectives for the Alpine Region,</p> <p>(b) a map showing existing and proposed types of development,</p> <p>(c) the performance criteria for development,</p> <p>(d) information about heritage items or places of heritage significance,</p> <p>(e) limitations on development on certain land, including environmentally sensitive areas, land prone to flooding and cultural heritage.</p>	noted
(3) The master plan may also contain proposals for infrastructure facilities, public utility undertakings, roads and transport.	noted
(4) The master plan must be consistent with this Chapter.	noted
(5) The Minister may amend or replace a master plan.	noted
(6) A draft master plan must be published on the NSW planning portal for at least 28 days before it is approved by the Minister.	noted
(7) A master plan approved by the Minister must be published on the NSW planning portal and takes effect on the day it is published.	noted

<b>4.27 Consultation with National Parks and Wildlife Service</b>	
(1) Development consent must not be granted to development in the Alpine Region unless the consent authority has— (a) consulted with the National Parks and Wildlife Service, and (b) considered submissions received from the National Parks and Wildlife Service within the relevant period.	Noted.  Consultation with NPWS will form part of the development assessment approval.
(2) In this section— relevant period means— (a) 28 days after notice of the development application is given to the National Parks and Wildlife Service, or (b) another period determined by the Planning Secretary.	noted
<b>4.28 Consideration of master plans and other documents</b>	
(1) In deciding whether to grant development consent to development in the Alpine Region, the consent authority must consider the following— (a) the aim and objectives of this Chapter set out in section 4.1, (b) a draft development control plan that is intended to apply to the land and has been published on the NSW planning portal, (c) a conservation agreement under the Environment Protection and Biodiversity Conservation Act 1999 of the Commonwealth that applies to the land, (d) the Geotechnical Policy —Kosciuszko Alpine Resorts published by the Department in November 2003, (e) for development in the Perisher Range Alpine Resort— (i) the Perisher Range Resorts Master Plan, published by the National Parks and Wildlife Service in November 2001, and (ii) the Perisher Blue Ski Resort Ski Slope Master Plan adopted by the National Parks and Wildlife Service in May 2002.	Complies  N/A. No DCP is in effect for the development site.  N/A  The proposed maintenance works will be occurring the existing external surfaces of the VT building and will not cause any ground disturbance. Hence, a geotechnical assessment is not required as part of this application.  N/A Works are not within Perisher Range.
(2) In deciding whether to grant development consent to development in the Alpine Region, the consent authority must consider— (a) a master plan approved by the Minister under section 4.26 that applies to the land, or (b) if a master plan has not been approved—a draft master plan prepared under section 4.26 that is intended to apply to the land and has been published on the NSW planning portal.	Complies
<b>4.29 Consideration of environmental, geotechnical and other matters</b>	
(1) In deciding whether to grant development consent to development in the Alpine Region, the consent authority must consider the following—	



<p>(a) measures proposed to address geotechnical issues relating to the development,</p> <p>(b) the extent to which the development will achieve an appropriate balance between—</p> <ul style="list-style-type: none"> <li>(i) the conservation of the natural environment, and</li> <li>(ii) taking measures to mitigate environmental hazards, including geotechnical hazards, bush fires and flooding,</li> </ul> <p>(c) the visual impact of the proposed development, particularly when viewed from the land identified as the Main Range Management Unit in the Kosciuszko National Park Plan of Management,</p> <p>(d) the cumulative impacts of development and resource use on the environment of the Alpine Subregion in which the development is carried out,</p> <p>(e) the capacity of existing infrastructure and services for transport to and within the Alpine Region to deal with additional usage generated by the development, including in peak periods,</p> <p>(f) the capacity of existing waste or resource management facilities to deal with additional waste generated by the development, including in peak periods.</p>	<p>N/A</p> <p>Complies. Existing</p> <p>No geotechnical works proposed. See Bushfire Report.</p> <p>Complies. Existing building. Deteriorated façade materials will be replaced like-for-like.</p> <p>N/A Existing</p> <p>The proposed development will not generate additional visitation to the resort and hence will have no impact on the existing infrastructure capacity and services for transport..</p> <p>Likewise, with no increase in visitation there will be no need to increase the capacity of existing waste disposal facilities, water supply or the reticulated effluent management system.</p>
<p>(2) For development involving earthworks or stormwater draining works, the consent authority must also consider measures to mitigate adverse impacts associated with the works.</p>	<p>No earthworks as proposed.</p> <p>There are no proposed changes to the existing stormwater drainage measures currently used for the VT building.</p>
<p>(3) For development the consent authority considers will significantly alter the character of an Alpine Subregion, the consent authority must also consider—</p> <ul style="list-style-type: none"> <li>(a) the existing character of the site and immediate surroundings, and</li> <li>(b) how the development will relate to the Alpine Subregion.</li> </ul>	<p>Replacement of like-for-like materials will result in no change to the existing character of the building.</p> <p>The proposed maintenance works aim to retain the original alpine character of the Thredbo Village. Hence, the proposed works will not significantly alter the character of the resort or Kosciuszko National Park.</p>
<p>4.30 Kosciuszko National Park Plan of Management</p>	
<p>(1) Development consent may be granted to development in the Alpine Region even if the application has not established that the development is consistent with the Kosciuszko National Park Plan of Management.</p>	<p>N/A The development is consistent with the Kosciuszko National Park Plan of Management.</p>
<p>(2) This section does not prevent the consent authority from refusing to grant consent to development on the basis that the development is not consistent with the Kosciuszko National Park Plan of Management.</p>	<p>noted</p>

Note—  
Under the National Parks and Wildlife Act 1974, section 81(4), operations on land to which a plan of management under that Act applies may be undertaken only if they are undertaken in accordance with the plan of management, despite another Act or an instrument made under an Act.

### 5.3 SNOWY MOUNTAINS SPECIAL ACTIVATION PRECINCT MASTER PLAN JULY 2022

#### 5.3.1 CHAPTER 10 ALPINE PRECINCT PROVISIONS

<b>Land Use (10.1) Performance Criteria</b>	
A. Development is to be permissible and consistent with the Master Plan, Precincts—Regional SEPP, Alpine Development Control Plan, Kosciuszko National Park Plan of Management, and the National Parks and Wildlife Act.	Complies. Alpine DCP is not yet in effect.
B. In considering the suitability of the development, the consent authority must be satisfied that the development meets the performance criteria and development controls in this Master Plan and in the Alpine Development Control Plan.	Complies with Master Plan. Alpine DCP is not yet in effect.
<i>Appropriate locations for alpine development</i>	
C. Development consent can only be issued for development in the Alpine Precinct where: i. the uses will support the diversification of the Alpine Precinct's tourism offering and year-round economic viability. ii. the uses will not compromise the environmental, heritage and cultural values of the Alpine Precinct. iii. the uses will not exceed the established carrying capacity of the Alpine Precinct.	Complies
D. The location of future development should align with the relevant structure plan and be focused on land marked 'Development area'. Where development is proposed on land outside these areas, additional technical investigation may be required.	N/A
E. Development for new or upgraded accommodation will meet the indicative sub-precinct yields and visitor thresholds set out in the Kosciuszko National Park Plan of Management and leasing arrangements. Refer also to Chapter 14 of this Master Plan	N/A
<b>Alpine Resorts (10.2) Performance Criteria</b>	
A. Development should contribute to visitor attraction and village experience through: i. the prioritisation of infill development. ii. improvements to pedestrian and active transport connections. iii. creation and implementation of active street frontages.	N/A Existing.
B. Development should integrate public transport opportunities and should create gateways and nodes to create a sense of place and community in Alpine Resort sub-precincts.	Existing



C. Development should provide a range of tourist accommodation offerings and seasonal worker accommodation.	Existing staff accommodation
D. Development should be designed to reduce on-site power consumption and improve environmental performance	Repairs to external linings will improve thermal efficiency of the building.
E. Development should be designed to contribute to the alpine character of the Alpine Resorts and reflect the alpine landscape and natural environment.	Complies. The proposed maintenance works will retain the original alpine character of the Thredbo Village
<b>Alpine Accommodation (10.3) Performance Criteria</b>	
These provisions are specific to development within the following Alpine Accommodation sub-precincts:• Ski Rider• Sponars Chalet • Thredbo Ranger Station• Creel Bay• Kosciuszko Tourist Park• Island Bend	N/A
<b>Alpine Experience (10.4) Performance Criteria</b>	
A. Public transport or mass transit connections should be integrated into the design of new developments, particularly in Alpine Resort and Alpine Accommodation sub-precincts.	N/A Existing development
B. Transport development must provide safe, reliable and accessible connections into and around the Kosciuszko National Park.	N/A
C. Development should be designed and staged to support and enable the ultimate growth of accommodation and attractions in the Alpine Region.	Complies.
D. Development of new and upgraded shared trails and paths should provide appropriate facilities and amenities.	N/A
E. Development should provide adequate car parking as part of a range of transport solutions (including the provision of accessible parking spaces).	N/A Existing
F. Visitor attractions must be supported by appropriate amenities, facilities and car parking and must minimise its impact to the natural environment.	Complies. Existing
G. Visitor attractions should be designed and staged to support and enable the ultimate growth of attractions in the Alpine Region.	Existing.

### 5.3.2 CHAPTER 11 ENVIRONMENT & SUSTAINABILITY

<b>Biodiversity (11.1) Performance Criteria</b>	
A. All development is to apply the avoid, minimise and offset methodology.	Complies. No change to existing footprint.
B. Development is to avoid threatened ecological communities and threatened species habitat; such vegetation should not be removed. Development may occur in these areas if it is for essential infrastructure	Complies. No change to existing footprint.
C. Development should be focused on colocation and infill to minimise biodiversity impacts	N/A
D. Development should be concentrated in and around already disturbed areas. Where possible, development should provide a buffer	Complies. No change to existing footprint.

between areas of high ecological value and buildings and structures.	
E. Development should consider the biodiversity impacts of bushfire asset protection zones (APZ) and associated vegetation management.	Complies.
F. Development must offset any impacts to biodiversity through direct management measures within Kosciuszko National Park and should be related to the biodiversity impacted	N/A biodiversity is not impacted.
G. Riparian corridors must be preserved while ensuring consistency with the proposed Flooding and Drainage Strategy for the Precinct.	The proposed repair works do not have the potential to negatively impact upon nearby terrestrial and aquatic habitats. The proposed works are minor in scale and nature and will be occurring to existing areas of the VT building only. Hence, the maintenance works ensure the protection of native flora and fauna within and surrounding the nearby Thredbo River.
H. Any revegetation or planting within Kosciuszko National Park should follow the Rehabilitation Guidelines for the Resort Areas of Kosciuszko National Park.	N/A
<b>Geotechnical (11.2) Performance Criteria</b>	
A. Development must address the requirements of the Geotechnical Policy – Kosciuszko National Park (DPNIR, 2003). This includes: i. development on land covered by the geotechnical maps, under the above policy must ensure the requirements of the policy are met. ii. development on land not covered by the geotechnical maps under the above policy must ensure the requirements of the policy are met and should also use the risk susceptibility mapping to inform the requirements and design of development	The proposed maintenance works will be occurring the existing external surfaces of the VT building and will not cause any ground disturbance. Hence, a geotechnical assessment is not required as part of this application.
B. Development must include an assessment of geotechnical risks.	No earthworks are being undertaken.
C. Buildings and structures must be designed to accommodate the specific geotechnical risks identified for the site	N/A Existing
D. Excavations required for new developments must consider the potential to cause widespread slope instability and ensure appropriate mitigation measures are implemented to minimise and manage risk.	N/A
<b>Flood Management Risk (11.3) Performance Criteria</b>	
A. The Flood Planning Level is the 1% AEP plus 500mm freeboard to ensure consistency across the Precinct. Development must generally occur outside the Flood Planning Level unless it can demonstrate that risks can be suitably managed. This allows for the maintenance of flood function and to avoid adverse effects on flood behaviour to the detriment of other properties or the environment of the floodplain	N/A
B. Development within the Flood Planning Level should demonstrate that: i. all structures are constructed with flood compatible building components below the 1%	N/A



AEP flood level plus 500mm freeboard. ii. all structures are designed to withstand the forces of floodwater, debris and buoyancy up to 1% AEP flood plus 500mm freeboard	
C. Development within the Probable Maximum Flood area should demonstrate that: i. all emergency and evacuation infrastructure is to be constructed with flood compatible building components below Probable Maximum Flood level plus 500mm freeboard. ii. all emergency and evacuation infrastructure structures are to be designed to withstand forces of floodwater, debris, and buoyancy up to Probable Maximum Flood plus 500mm freeboard. iii. development must be sited, designed and located to avoid or mitigate the flood risk to people, property and infrastructure such that: <ul style="list-style-type: none"> <li>flood risk is managed through site-specific built form and design.</li> <li>sensitive, vulnerable and critical uses are avoided in the floodplain.</li> </ul>	N/A
D. Development should mitigate the impacts of local overland flooding through the provision of adequate site drainage systems, where possible.	N/A
E. Development must consider and plan for emergency evacuation situations to ensure the safety of all areas within the Probable Maximum Flood extent.	N/A
<b>Water Quality (11.4) Performance Criteria</b>	
A. Maintain or improve the ecological condition of waterbodies and their riparian zones in catchments over the long term	<p>The subject site does contain areas identified as “Riparian Lands and Watercourses”, as the VT building is located to nearby Thredbo River.</p> <p>The South-western most corner of the VT building and approximately half of the Guest Services building is located within the Riparian corridor mapping.</p> <p>However, the proposed works are for maintenance works only and will not cause ground disturbance. The proposed works do not have the potential to negatively impact upon the functioning and quality of the riparian corridor based on the minor scale and nature of the works.</p> <p>Waste should be handled carefully and disposed of correctly on site to avoid any contamination of the local waterway.</p> <p>Please refer to Waterfront Land E-tool search result for the development in Appendix I that demonstrates that the proposed works do not require a Controlled Activity Approval Licence.</p>
B. Development in the Alpine Precinct should implement on-site water management and water quality systems through: i. the capture and re-use of water on-site. ii. the treatment of water on-site with any water discharged back into catchments having a	Existing on-site water management and water quality systems will not be altered as part of this application.

neutral or beneficial effect on water quality. iii. incorporating water sensitive urban design principles into the development's-built form and landscaping, where possible.	
C. The quality of stormwater discharged into receiving catchments must be pre-development quality or better in relation to pH, total suspended solids, total phosphorus, total nitrogen and gross pollutants. The quality of water should aim to meet the following targets: i. Total Suspended Solids: 85% reduction. ii. Total Phosphorus: 60% reduction. iii. Total Nitrogen: 45% reduction.	Existing stormwater systems will not be altered as part of this application.
D. The quality of water discharged into receiving catchments should maintain electrical conductivity levels. Water quality should aim to maintain an electrical conductivity below the 30 $\mu$ S/cm ANZG 2018 Guideline value for upland rivers of South-East Australia	No change
E. Monitor macroinvertebrates to ensure they are consistently within Band A of the NSW AUSRIVAS model.	N/A
F. Erosion and sediment control should be managed during construction to ensure impacts to waterways are minimised in accordance with Managing Urban Stormwater Soils and Construction, also known as the Blue Book (current edition)	N/A no earthworks are proposed.
G. Discharge of wastewater and/or contaminated stormwater to watercourses or waterways is not permitted unless other specified in an environmental protection licence issued under the Protection of the Environment Operations Act 1997. Development must obtain the appropriate water licenses in accordance with the Water Act 1912 and the Water Management Act 2000 and consider the relevant Water Sharing Plan	Noted.
<b>Bushfire (11.5) Performance Criteria</b>	
A. Development is to: i. minimise perimeters exposed to the bushfire hazard. ii. minimise vegetated corridors that permit the passage of bushfire towards development. iii. provide for the siting of future development away from ridge-tops and steep slopes, within saddles and narrow ridge crests. iv. ensure capacity of existing infrastructure (such as roads and utilities) can accommodate the increase in demand during emergencies as a result of the development.	The subject site is identified as being in bushfire prone land, and hence as outlined in Planning for Bushfire Protection – PBP 2019 (NSW RFS 2019) is considered Special Fire Protection Purpose (SFPP) and is required to obtain a BFSA from the RFS under section 100B of the RF Act. See BHAR submitted as part of this application.
B. Asset Protection Zones are to be provided and maintained between a bushfire hazard and future development and are designed to address the relevant bushfire attack mechanisms	There is an existing appropriate defensible space surrounding the proposed development. The Valley Terminal area in Thredbo has substantial separation from the nearest bushfire threat, this allows emergency service personnel and staff to undertake property protection activities in the area to limit the potential spread and impact of bushfire. For this reason, an APZ is not required for the proposed development,



	as Managed Land meets the requirements of an APZ.
C. Adequate access is to be provided from all properties to the wider road network for park users emergency services and to provide access to hazard vegetation to facilitate bushfire mitigation works and fire suppression	Friday Drive is a two-wheel drive, all weather road. The widths and design would allow safe access for firefighters while residents are evacuating an area. The capacity of road surfaces is sufficient to carry fully loaded firefighting vehicles.
D. Development is to minimise levels of radiant heat, localised smoke and ember attack through development design and siting	Minor non-structural (external) building repair works do not have any influence on potential bush fire impacts and the bush fire protection of the building (section 6.5 PBP 2019). The repair and replacement cladding, roof sheeting and windows, will be like-for-like replacement with an equivalent or improved material.
E. The subdivision of land and location of developments should consider the future uses of land and the inclusion of roads into Asset Protection Zones.	N/A
<b>Sustainability &amp; Climate Change (11.6) Performance Criteria</b>	
A. Development must be inclusive and sustainable and promote year round use.	Complies.
B. Development should preserve the Precincts landscape, cultural, heritage and biodiversity values by avoiding and minimising impact.	Impacts on the natural environment will be minimal as no excavation or ground disturbance will occur as a result of the proposed maintenance works. The implementation of appropriate environmental controls during construction will limit the potential impacts from the works on the existing natural environment.
C. Development should support sustainable and active transport opportunities and integrate open space. Buildings are to express a strong commitment to ESD principles and incorporate passive design, optimal orientation, effective sun shading, cross ventilation and open plan living. This should be evident in the external architectural expression.	Existing
D. Development should comply with applicable sustainability tools and programs for design, construction and operation.	Complies.
E. Consideration must be given to climate responsiveness and resilience. Climate change risks, hazard and opportunities must be considered in the design, construction and operation of development within the Precinct.	Consideration of materials has been made for the project.
F. Operators, lessees and licensees within the Precinct must prepare and maintain an Environmental Management System in accordance with ISO14001:2015 – Environmental management systems and the requirements of the Plan of Management for Kosciuszko National Park	Noted

### 5.3.3 CHAPTER 12 PLACE & LANDSCAPE

<b>Aboriginal Cultural Heritage (12.1) Performance Criteria</b>	
A. Areas of Aboriginal cultural heritage (included as part of the environmentally sensitive areas map) should not be developed. Development may occur in these areas if it is for essential infrastructure and where further Aboriginal	Works are limited to the exterior of the existing building. No ground works are proposed.

cultural heritage assessment will be undertaken to appropriately mitigate and manage any impacts to Aboriginal cultural heritage items, places or areas.	
B. Aboriginal culturally significant places and sites should be integrated with areas of environmental significance and green space (where appropriate) across the Precinct. This may continue to evolve as greening opportunities across the Precinct are established	Noted.
C. Development in areas where surveys have not been undertaken require further Aboriginal cultural heritage assessment. These assessments must be carried out in accordance with Guide to investigating, assessing and reporting on Aboriginal cultural heritage in NSW (as modified from time to time) prior to any development on this land. These assessments must include a visual survey of the land. Once suitably assessed, any land identified as having Aboriginal cultural heritage significance should be included on the Environmentally Sensitive Areas (ESA) map. Development is to be assessed against the mapped zones of archaeological potential as required by the following: i. development within areas identified as 'disturbed land' do not require any further investigation beyond considering the potential for subsurface archaeological deposits. If current disturbances are considered to cover intact archaeological deposits, further investigation should take place that may include test excavation. Should development encounter any unexpected finds during construction, the procedures under the relevant unexpected finds protocol should be followed. ii. works within areas identified as "moderate ACH potential' or 'high ACH potential' should be avoided. Where development will impact these areas, further Aboriginal cultural heritage assessment must be undertaken. This assessment should include a visual inspection, possibly test excavation if warranted, and participation from the Aboriginal community	N/A
D. Development planned on land in which an Aboriginal object is located should be supported by a heritage impact assessment which should be prepared to assess the extent to which a proposed development would harm Aboriginal objects.	N/A
E. If impact to an Aboriginal object is unavoidable, an Aboriginal Heritage Impact Permit (AHIP) under Part 6 of the National Parks and Wildlife Act 1974 would be required.	Noted.



<b>Historic Heritage (12.2) Performance Criteria</b>	
A. Development in areas defined as 'disturbed land' can occur without further historic heritage investigation however must consider neighbouring heritage items and broader heritage values.	Noted.
B. Development on land where a heritage item is situated, that is a heritage item or is on land adjacent to a heritage item must prepare a statement of heritage impact.	
C. Development in areas defined as 'high risk' or 'moderate risk' requires further heritage assessment where the development is likely to materially have a major affect on a heritage item or its value. Development is considered to have a materially major affect if it involves: <ul style="list-style-type: none"> <li>i. the full or partial demolition of a building.</li> <li>ii. major alterations or additions.</li> <li>iii. major adverse impacts, such as the removal of significant fabric, obscuring key views or dominating a heritage item, or the removal of evidence of significant historical associations; &amp;</li> <li>iv. impact to significant archaeological deposits.</li> </ul>	N/A
D. Development in areas defined as 'high risk' or 'moderate risk' requires further heritage assessment where the development is likely to materially have a minor affect on a heritage item or value. Development is considered to have a minor affect if it involves (but is not limited to): <ul style="list-style-type: none"> <li>i. repairs or restoration to fabric.</li> <li>ii. installation of fire safety equipment.</li> <li>iii. installation of disabled access.</li> <li>iv. replacement of awnings, balconies, etc.</li> <li>v. installation of signage or fencing.</li> <li>vi. excavation of areas without archaeological potential.</li> <li>vii. erection of temporary structures.</li> <li>viii. installation of safety and security equipment.</li> </ul>	See Biodiversity, Aboriginal & Cultural Assessment.
E. Where development is likely to materially have a major effect on a heritage item or value, further heritage assessment is required. This heritage assessment includes: <ul style="list-style-type: none"> <li>i. a visual inspection to determine the existing heritage values.</li> <li>ii. an archaeological assessment (if appropriate).</li> <li>iii. preparation of a statement of heritage impact.</li> </ul>	The removal and replacement of existing deteriorated native hardwood cladding, metal roof sheeting and windows and replacement with like-for-like products will not impact the heritage value of the building. The signage change to TV Screens on the Guest Services building will have an acceptable heritage impact.
F. Where development will have minor effect on a heritage item or value, a heritage assessment may be required. This heritage assessment may include: <ul style="list-style-type: none"> <li>a visual inspection to determine the existing heritage values.</li> <li>i. an archaeological assessment (if appropriate).</li> <li>ii. use of a previously prepared heritage study if applicable.</li> </ul>	The removal and replacement of existing deteriorated native hardwood cladding, metal roof sheeting and windows and replacement with like-for-like products will not impact the heritage value of the building.  Please refer to the following documents prepared by GBA Heritage Consultants for the modified development approval.

G. Development that is likely to have a materially major or minor effect on a heritage item or its value must: i. identify the impacts to the heritage values of an item or place. ii. demonstrate the need for the impact and how alternatives to the impact have been considered. iii. demonstrate how the adverse impacts will be minimised or mitigated.	N/A
H. Development adjacent to a heritage item should ensure impacts to the heritage item are minimised, including through the provision of appropriate curtilages. There may be opportunities to reduce the curtilage to some heritage items if it can be demonstrated the development will not have a significant impact on the heritage item or its value.	N/A
I. Heritage items must be used for purposes that are appropriate to their heritage significance, including adaptive re-use where appropriate.	No change in use is proposed.
J. Development is to ensure long-term heritage conservation outcomes are retained or interpreted to reflect the history of heritage items and places.	Replacement of deteriorated external materials will ensure the longevity of the building.
K. Development should through redevelopment or upgrades remove inappropriate or unsympathetic alterations and additions to heritage items and reinstate significant missing details and building elements, where possible.	Noted. N/A
<b>Landscape, Character &amp; Open Space (12.3) Performance Criteria</b>	
A. Development should be designed to sensitively integrate into the landscape and should respond appropriately to the topography and climate of the Alpine Precinct	Existing
B. Development should protect, conserve and enhance the Alpine Precinct's natural environment and create a green infrastructure network, where possible.	N/A Existing
C. Landscaping and public open spaces should include plantings of native species found in surrounding plant communities, which aim to achieve the re-establishment of biodiversity in addition to aesthetic appeal and enhancement of the functionality of an area.	Landscaping is existing.
D. Revegetation and new plantings should follow the Rehabilitation guidelines for the Resort Areas of Kosciuszko National Park	No revegetation is proposed.
E. Development should integrate stormwater management infrastructure with open spaces, where possible.	Existing.
<b>Built Form (12.4) Performance Criteria</b>	
<b>General criteria for all development in the Alpine Region</b>	
A. Buildings should be efficient, well designed and successfully integrated with the surrounding landscape. This will be achieved by: i. ensuring building bulk, orientation and design contributes to the energy efficiency of buildings, particularly with respect to thermal comfort. ii. ensuring new buildings are located within	The proposed maintenance works will not alter the apparent mass and bulk of the existing VT building.



<p>existing disturbed areas to minimise impacts on vegetation and natural processes.</p> <p>iii. siting development within existing disturbed areas to limit clearing and the expansion of new development areas.</p> <p>iv. incorporating climate resilient design principles in new development.</p> <p>v. applying suitable rehabilitation and native landscaping.</p> <p>vi. incorporating preparedness for natural hazards and climate change into development design.</p> <p>ensuring development creates activated public domain spaces and provides safe and accessible pedestrian connections between buildings, appropriate for all seasons</p>	
B. Site earthworks must respond to local topography and geotechnical characteristics and be appropriate for the intended land use.	N/A no earthworks proposed.
<b>For village centres and public domain</b>	
A. Development should create an integrated streetscape where active frontages promote movement between the private and public realms.	N/A existing
B. Building entries should connect to an accessible (providing equitable access to all pedestrians) pedestrian network through design features, wayfinding, and landscape treatments	Complies
C. Development should integrate and provide public seating, shelter and lighting to contribute to increased activity and safety in the public realm	Existing
D. Development should provide human-scale buildings ensuring building envelopes allow adequate solar access and views, including ensuring significant views to natural features are protected	The proposed maintenance works will not change the design, scale and footprint of the existing building. Hence, there is no anticipated impact to the privacy of occupiers and users of other land. Furthermore the proposed works will not impact the solar access available to recreational users of the alpine resort.
E. Development should provide for year-round weather protection that reduces the impacts of wind and snow accumulation in winter and provides adequate shade in summer	Existing. The roof design of the VT building is appropriately sloped, and directs accumulated snow to fall away from entrances, establishing safe entry and exit into and out of the VT building.
F. Development should provide clearly defined and separate pedestrian and vehicle entries to minimise conflicts.	Existing
G. Development should allow for snow clearing and adequate interface with oversnow vehicles, where appropriate.	Existing

### 5.3.4 CHAPTER 13 TRANSPORT & INFRASTRUCTURE

<b>Transport Network (13.1) Performance Criteria</b>	
<p>A. Transport infrastructure should integrate the public transport network with the existing road network by:</p> <p>i. ensuring public transport stops are strategically located and provide adequate all-weather shelter and accessibility.</p>	Existing

ii. minimising vehicle conflict with active transport and public transport routes	
B. Development must provide operational access and egress for emergency services and occupants	Existing
C. Development should integrate active transport connections that promote movements between the Alpine resorts, where possible	Existing. The proposed works will not result in an increase of visitors to the resort.
D. New development must provide and integrate new technologies, such as electric vehicle charging and electronic checkpoints, where possible	N/A. No electrical works are proposed as part of this application for external cladding.
<b>Utilities, Services &amp; Infrastructure Performance Criteria</b>	
A. Development within the site must have access to water, wastewater, digital connectivity and telecommunications, energy and drainage infrastructure.	Complies
B. Utilities and services must be integrated with existing infrastructure and services, where possible.	Existing
C. Utilities and services should be integrated into road reserves, active transport corridors or the public domain, where possible.	Existing
D. Infrastructure and services must be designed to provide for the ultimate growth and development in Alpine Resorts.	Existing
E. Development should provide and integrate water cycle management and renewable energy solutions into the design of buildings and structures, where possible	Existing

### 5.3 OTHER APPROVALS RURAL FIRES ACT 1997

The subject site is identified as being in bushfire prone land, and hence as outlined in *Planning for Bushfire Protection – PBP 2019* (NSW RFS 2019) is considered Special Fire Protection Purpose (SFPP) and is required to obtain a BFSa from the RFS under section 100B of the RF Act. See bushfire hazard assessment report submitted as part of this application.

### 5.4 ENVIRONMENTAL PLANNING AND ASSESSMENT ACT 1979 (203)

#### 5.4.1 OBJECTS OF THE EP&A ACT

In determining a development application, a consent authority must take into consideration the matters referred to in Clause 4.15 (1) of the EP&A Act as are of relevance to the development:

<b>4.15 – 1 (a) (i) the provisions of an environmental planning instrument</b>
The applicable environmental planning instrument is State Environmental Planning Policy (Precincts—Regional) 2021.
<b>4.15 – 1 (a) (ii) the provisions of any proposed instrument that is or has been the subject of public consultation under this Act and that has been notified to the consent authority (unless the Planning Secretary has notified the consent authority that the making of the proposed instrument has been deferred indefinitely or has not been approved)</b>
None are applicable to the proposal.
<b>4.15 – 1 (a) (iii) the provisions of any development control plan</b>
None are applicable to the proposal.



4.15 – 1 (a) (iia) the provisions of any planning agreement that has been entered into under section 7.4, or any draft planning agreement that a developer has offered to enter into under section 7.4
None are applicable to the proposal.
4.15 – 1 (a) (iv) the provisions of any Regulations (to the extent that they prescribe matters for the purposes of this paragraph)
None are applicable to the proposal.
4.15 – 1 (b) the likely impacts of that development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality
<p><b>NATURAL ENVIRONMENT:</b> The proposed works are not anticipated to have any negative impacts on the surrounding natural environment.</p> <p><b>BUILT ENVIRONMENT:</b> The proposed maintenance works will improve the condition of the VT building to ensure its longevity. The works will not involve any changes to the design, scale or footprint of the existing VT building. The proposal aims to retain the original alpine character and fabric of the Thredbo Village. Hence, the overall change to the built environment is considered of positive influence on the existing building and locality.</p> <p><b>SOCIAL AND ECONOMIC IMPACTS IN THE LOCALITY:</b> The proposed works have been designed to minimise any amenity impacts on the neighbouring buildings, they are not expected to generate any negative social or economic issues.</p>
4.15 – 1 (c) the suitability of the site for the development
<p>The proposed maintenance works will improve the condition of the VT building, and are deemed as suitable for the subject site.</p> <p>The subject site is identified as being in bushfire prone land, and hence as outlined in <i>Planning for Bushfire Protection – PBP 2019</i> (NSW RFS 2019) is considered Special Fire Protection Purpose (SFPP) and is required to obtain a BFSA from the RFS under section 100B of the RF Act. See BHAR submitted as part of this application.</p>
4.15 – 1 (d) any submissions made in accordance with this Act or the regulations
The application does not require notification to adjoining landowners, as Kosciuszko Thredbo as the applicant, is also the Head Lessee.
4.15 – 1 (e) the public interest
The development proposal satisfies the objectives of the State Environmental Planning Policy (Precincts—Regional) 2021 and is considered positive in terms of the public interest.

## 6.0 CONCLUSION

### SUITABILITY OF THE SITE FOR DEVELOPMENT

The proposal complies with the relevant environmental planning instruments and policies. The Statement of Environmental Effects confirms that the site is suitable and capable of sustaining the proposed development, with no adverse impacts.

#### Submissions Made in Accordance with the Act or regulations:

Given the proposals minimal environmental impact it is unlikely to raise significant objection.

#### The Public Interest

The proposal is in the public interest as:

- It provides a sustainable land use.
- The proposal is suitable within the locality.
- The proposal is positive in terms of the amenity of the area.

## 7.0 SITE ENVIRONMENTAL MANAGEMENT PLAN (SEMP)



### Site Environmental Management Plan

#### Project: Valley Terminal External Building Alterations

##### PROJECT OVERVIEW

The works will comprise:

- The removal of existing deteriorated native hardwood cladding and replacement with like-for like product.
- The removal of existing deteriorated corrugated metal roof sheeting and replacement with like for-like product. It is proposed that the roof sheets that need replacing will be done as required.
- The removal of windows and replacement with like-for-like product on the main building (excludes windows that have recently been replaced e.g. the large window above Avalanche). It is proposed that the windows that need replacing will be done as required.
- Reinstatement of five (5) ticket windows on the Valley Terminal Guest Services building.
- Replacement of deteriorated timber cladding on the underside of the awning on Valley Terminal Guest Services building.



Figure 1: Site plan





### CONSTRUCTION TIMING

It is anticipated construction will commence sometime during the summer 2023/24 construction period as stipulated in the conditions of consent. The works will be carried out over a 5 year period during the summer construction period.

Generally, the alpine resorts construction period commences after the October long weekend and ends no later than 30 April the following year, unless otherwise approved. Construction is planned to be undertaken over several summers.

### PUBLIC SAFETY & EXCLUSION AREAS

All buildings will be occupied during construction. Site measures for the protection of works, staff and public during construction will include:

- Construction notification via email to all staff/occupants / owners of premises, including contact details for the construction contractor.
- If staff become disrupted by construction noise at any time, KT will organise alternative working arrangements, such as working from home or relocating to another office space.
- Construction deliveries will be timed outside of peak hours where possible to minimise disruptions to staff and the general public in the locality.
- Temporary site fencing will be erected around the works area to exclude public access. Fencing is to clearly delineate the construction area and keep the disturbance area to a minimum to restrict unauthorised persons entering the work area.
- Exclusion and directional signage will be erected to manage the public around the works area.

The use and management of plant, scaffolding and vehicular movements will be managed by KT (or its contractors) in accordance with industry standards.

Demolition works must comply with the provisions of *Australian Standard AS 2601-2001 Demolition of structures*.

All scaffolding is to be located with the lot boundary and shall comply with *AS/NZS 1576 Scaffolding* and *AS/NZS 4576 Guidelines for Scaffolding*.

### BUILDING MATERIAL STORAGE LOCATIONS

Temporary stockpiling of building materials will be required during construction. Fencing and signage will be erected around these locations to ensure public are excluded.

The proposed material storage locations (**Figure 2**) are located outside of the main public thoroughfares to minimise disruptions to the retail shops, offices and food and beverage providers within the site. The waste storage location is on relatively flat disturbed grassland. The temporary building material storage is located on a hardstand/paved surface.



Figure 2: Temporary Storage Locations

#### DUST CONTROL

Construction is not expected to create any unnecessary air pollution. The following dust control measures will be implemented:

- All vehicles carrying materials or waste to/from the site must be covered;
- Covers are to be adequately secured; and
- Footpaths and roads adjacent to the site are to be kept clean.

#### CHEMICAL SPILL PREVENTION AND CONTAINMENT

The proposed development will not require the storage or use of any hazardous materials. However, if any petroleum products, trade waste, garbage and other noxious substances are required for the subject works, although unlikely to be required in any substantial amount, this will be appropriately stored off-site.

#### NOISE & VIBRATION

Working hours will be stipulated in the conditions of consent. Hours for construction will be determined in order to allow for construction on suitable days during appropriate hours to prevent any negative impacts on the amenity of visitors and employees.

Noise on a construction site can become a form of pollution to the local environment through the use of plant, machinery and tools. Staff to be issued with appropriate PPE.

To reduce noise pollution from site the following procedures will be followed:





- All plant, machinery and tools will be maintained in good working order at all times;
- Work involving noisy tools or machinery to be used inside the building structure when possible;
- Strict hours of operation for each site will be implemented to reduce noise pollution to the surrounding areas
- In the instance of receiving a complaint in regards to noise levels immediate rectification will occur as far as practical.

Construction works are to be managed in accordance with *Australian Standard 2436-2010 Guide to noise and vibration control on construction, demolition and maintenance sites*.

#### CHEMICAL SPILL PREVENTION AND CONTAINMENT

The proposed development will not require the storage or use of any hazardous materials. However, if any petroleum products, trade waste, garbage and other noxious substances are required for the subject works, although unlikely to be required in any substantial amount, this will be appropriately stored off-site.

#### INDIGENOUS HERITAGE

Should any material suspected of being an Aboriginal object become unearthed in the course of works associated with the proposed works, all work at that location shall cease immediately as per Section 90 of the *National Parks and Wildlife Act 1974*, and the Office of Environment and Heritage shall be contacted immediately to arrange for representatives to inspect the site.

#### WASTE MANAGEMENT

No ground disturbance will occur within 40m of the Thredbo River, however caution must be taken when disposing of and handling waste material on site, to prevent any runoff of debris into the river. Building waste will be temporarily located at the western end of the building before removal offsite. All waste generated from the proposed works, and the general construction waste and other waste associated with the repair and maintenance work will be disposed of in a skip bin, that will then be deposited to an external waste facility.

To ensure that waste is managed appropriately, the following controls and measures are to be adhered to:

- Building waste must be minimised and must be contained in receptacles and covered daily or removed from the site each day. Receptacles are to be protected to ensure waste cannot escape by wind, water or scavenging fauna.
- To the furthest extent possible efforts shall be made to reduce, reuse and recycle materials used onsite. Wherever possible, salvage building material for re-use.
- A daily inspection shall be carried out to ensure the worksite is left in a rubbish free state.
- All employees shall be informed of the need to maintain a clean worksite.
- Site generated waste shall be collected and removed from the site upon completion.
- All loads of rubbish removed shall be securely covered to ensure no spillage.
- The worksite shall be left in a tidy and rubbish free state upon completion of the project.

Litter control around the site is the responsibility of all construction staff. A daily site clean up to reduce litter around the site and prevent any possible hazards it causes will be performed. Litter collected can be placed in bins or specified area (Figure 1) and disposed of at the local council tip.



## TRAFFIC MANAGEMENT

Construction vehicles and machinery will enter the site via the bridge from Friday Drive. Construction vehicles and machinery will park adjacent to the site (eastern end of the building) to minimise disruption within the locality.

Traffic management measures will include the erection of signage and fencing (as required) to:

- Minimise effects on traffic movements and amenity;
- Manage and control vehicular movements to and from the site;
- Maintain public access through/around the site; and
- Maintain safety for workers and the public.

## REHABILITATION

The material storage area identified in **Figure 2** is to be rehabilitated upon completion of works in accordance with the Rehabilitation Guidelines for the Resort Areas of Kosciuszko National Park (NGH 2007).

## EMERGENCY PROCEDURES

All Project personnel are required to follow KT's **Construction site Incident and Emergency Procedures Thredbo Village 2021/2022**. The procedure will be available on-site and all Project staff will be trained on their implementation through the site induction. The procedure classifies examples of emergencies and incidents and provides specific procedures for response to such events, such as:

- Serious injuries requirement urgent medical help.
- There are threats to property or life.
- Criminal activity e.g. you have witnessed a serious crime or accident.
- Sewer or water service breaks.
- Bushfire, building fire, spot fire on-site.
- Electricity service faults.
- Leaking gas.
- Fires and explosions.
- Release of pollution e.g. release of sediment into watercourse, chemical spill.

The procedure also outlines general site management principles, incident reporting and notification requirements and provides an emergency contacts list.

External contractors are required to prepare and implement an emergency and incident response procedure. The contractor will be responsible for responding to any environmental emergency caused by any action (or inaction) of the contractor's staff, including notification requirements to external parties such as EPA and Fire, Fire and Rescue NSW.

In the event of an environmental incident, emergency or near-miss, the following steps should be taken:

- **STOP** works in the area and if safe to do so ensure the safety of personnel within the vicinity.
- **NOTIFY** relevant persons e.g. emergency services or Construction Manager.





- **ISOLATE** the risk or hazard e.g. turn off machinery/plant, implement immediate site controls, set up exclusion zone. and
- **REPORT** and notify relevant persons (e.g. Project Manager, regulatory agencies).

Environmental incident and near-miss reporting requirements are detailed below.

#### ENVIRONMENTAL INCIDENT REPORTING

All incidents and near misses will be managed in accordance with KT's **Construction site Incident and Emergency Procedures Thredbo Village 2021/2022**. The document provides procedures for responding to incidents and emergencies, reporting and notification requirements and emergency contacts.

The following information should be recorded:

- Time and date of the incident / near miss
- A description of the incident / near miss
- A sequence of events that led to the incident / near miss occurring
- Person/s involved in the incident / near miss (including witnesses)
- Written statements from person/s involved (as applicable)
- Details of corrective actions.

The **Environmental Incident Report Form** should be completed for all environmental incidents. All parts of the form must be completed in accordance with KT's incident procedure and following the instructions within the form. The form must be signed by the person making the report and the Project Manager/person in charge of the site/activity.

#### KEY CONTACTS

Key contacts are listed in **Table 1**.

Table 1: Key Contacts

Company / Agency	Role / Reason	Contact
Department of Planning and Environment (DPE) (Alpine Resorts Team)	Development approval and compliance	(02) 6456 1733
National Parks and Wildlife Service (NPWS)	Flora, fauna, archaeology	(02) 6450 5600
Environment Protection Agency (EPA)	Water, noise, air pollution and regulation	131 555
NSW Soil Conservation Service	Soil erosion and sediment control	02 9842 8300
Thredbo Medical Centre	General medical attention	(02) 6457 6254
Fire and Rescue Thredbo, NSW	Incident / emergency	(02) 6457 6144
NSW Police	In case of fire, medical or police emergency	000
NSW Fire and Rescue		
NSW Ambulance		

## APPENDIX I WATERFRONT LAND E-TOOL SEARCH RESULT

### Question 1 - Department of Planning and Environment—Water waterfront land maps

Is your property located on a watercourse, lake or estuary within the area marked in orange in any of the Department of Planning and Environment —Water waterfront land maps below? \*

- ☐ Yes, Botany Bay
- ☐ Yes, Brisbane Water
- ☐ Yes, Hunter River
- ☐ Yes, Lake Macquarie
- ☐ Yes, Lake Mulwala
- ☐ Yes, Port Hacking
- ☐ Yes, Port Jackson
- ☐ Yes, Port Stephens
- ☐ Yes, Tuggerah Lakes
- ☐ Yes, Wallis Lakes
- ☒ No, none of the above

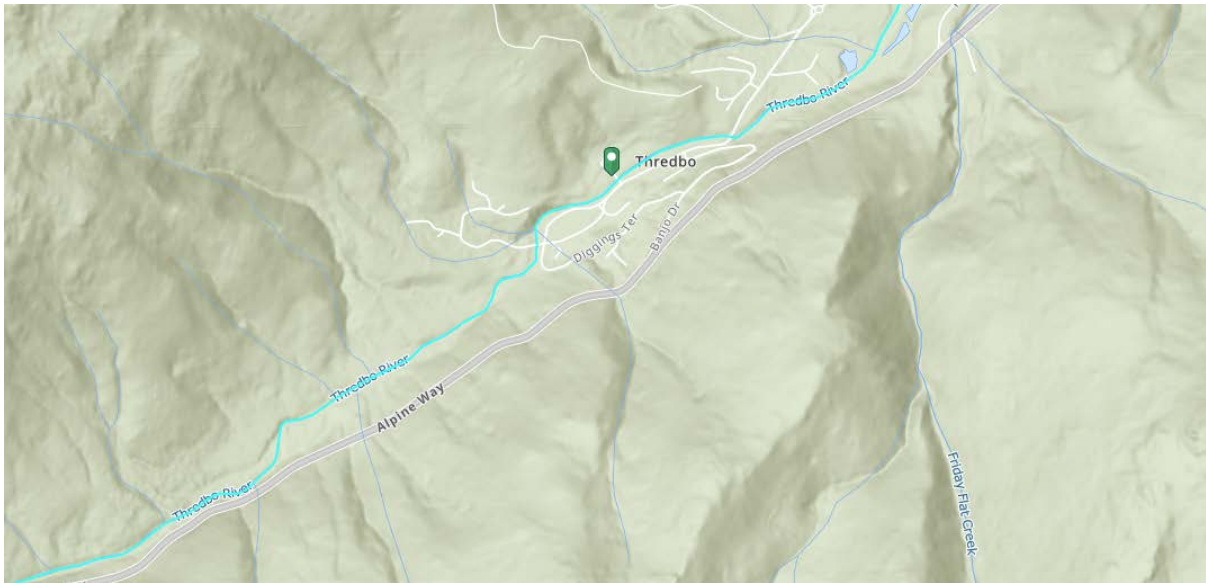
### Question 2a - Hydro Line spatial data map

Is there a blue line on your property or within 40m of the proposed work? \*

- ☒ Yes
- ☐ No



### Question 3 - Determining stream order



What is the stream order? \*

- ☒ 1st or 2nd order stream
- ☐ 3rd order or greater stream

### Question 4a - Watercourse defined bed or bank

Does the watercourse have a defined bed or bank?

- ☒ Yes
- ☐ No

### Question 4b - Department of Planning and Environment—Water Guide - Watercourse types

- ☐ Type 1
- ☐ Type 2
- ☐ Type 3a
- ☐ Type 3b
- ☐ Type 3c
- ☐ Type 4
- ☒ Type 5

### Question 5c - Watercourse features

Using the photos below, are there any watercourse features present? \*

- ☐ Yes
- ☒ No

### Result 12 - Controlled activity approval not required - No watercourse

Based on your answers, the result is:

**CONTROLLED ACTIVITY APPROVAL NOT REQUIRED - NO WATERCOURSE**